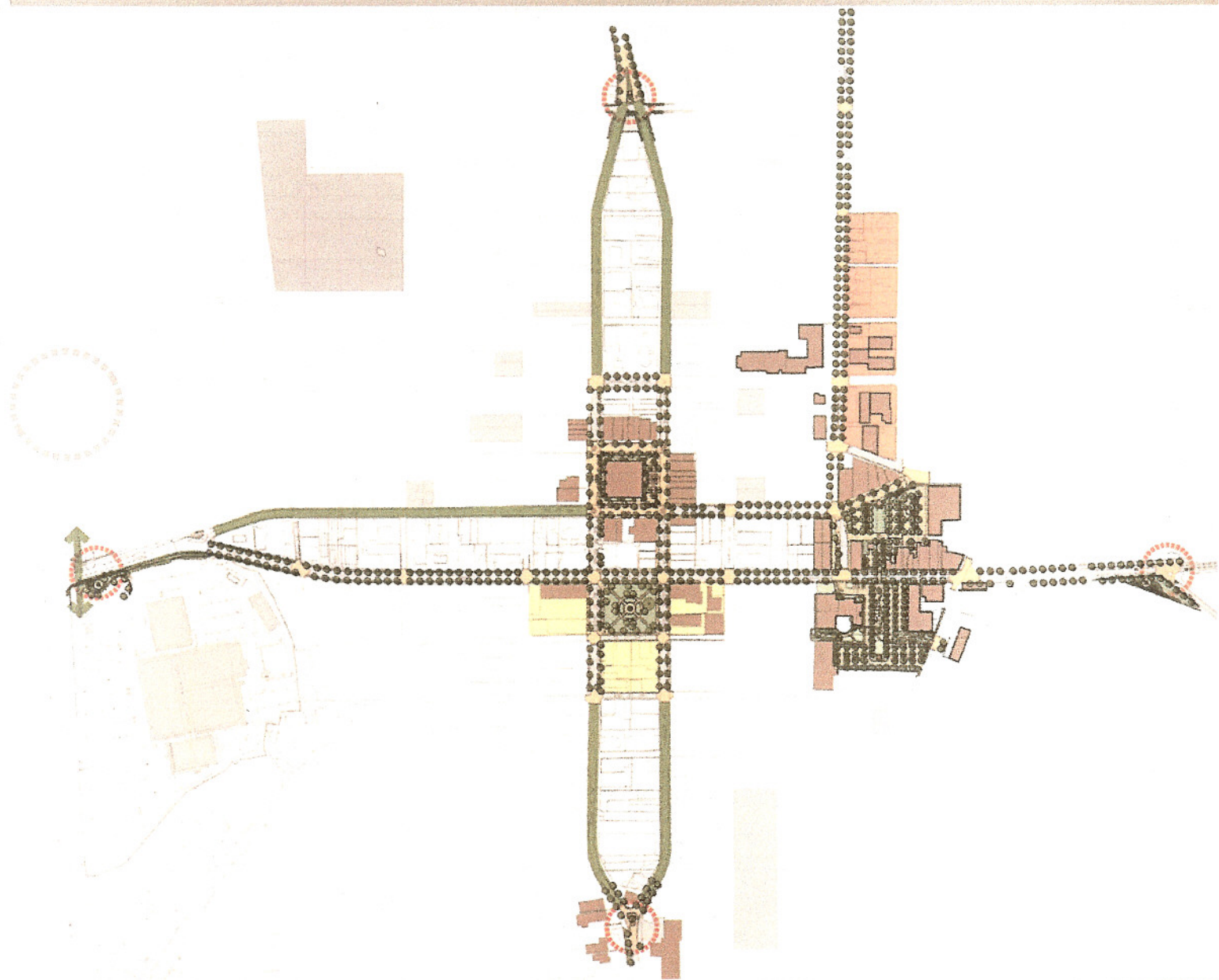


DOWNTOWN CLEVELAND



MASTER PLAN

CLEVELAND, TENNESSEE

2004

DOWNTOWN CLEVELAND

The Downtown Cleveland Master Plan was made possible through the generous financial support of the following:

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MASTER PLAN CLEVELAND, TENNESSEE 2004

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INTRODUCTION

In the spring of 2003 Mainstreet Cleveland, composed of city/county officials, volunteers and business people, focused on the goal of making downtown Cleveland a more vibrant place to work, live, shop and entertain.

After initial consultations, the Mainstreet Cleveland group decided to develop a master plan that would provide direction for the revitalization of downtown Cleveland. In order to achieve these goals, Mainstreet Cleveland collaborated with landscape architects/architects from Ross/Fowler, P.C. and their consultants to develop a Master Plan for downtown Cleveland. The plan is a policy document and guide for city/county officials, developers and other decision makers with regard to the current and long-range protection, enhancement, growth and development of downtown Cleveland.

“The vision points where you want to go. The master plan provides a map showing you the way”

GOALS & PURPOSE

The driving force behind this master planning process has been to develop an implementable plan that responds to the needs and concerns of downtown Cleveland. It is a collective expression of how the community views itself and what it would like to become. Thus, the purpose of this document is to serve as an action-based strategic plan that will guide the citizens of Cleveland in taking steps to revitalize their downtown by improving their entrances (gateways and portals), enhance the current downtown (streetscapes and public amenity spaces) and to generate new development activity (public and private).

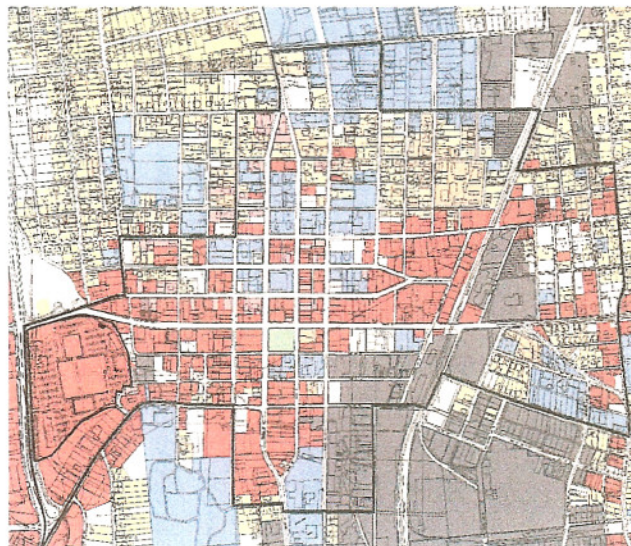
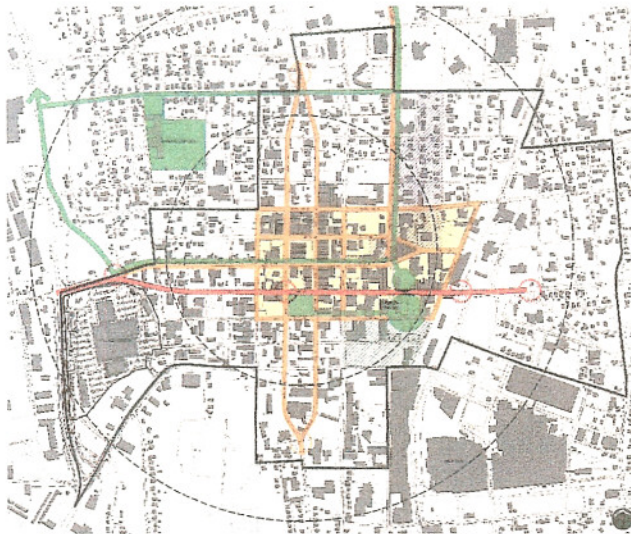
It is essential for Cleveland, both in relation to its health and heritage, to recapture and revitalize the Downtown into a beacon of energy and economic activity that will attract people to the city.

A series of design goals have been developed to help make downtown Cleveland a more desirable destination by creating a stronger visual and cultural identity by addressing the following issues.

1. Strengthen the downtown identity and sense of place.
2. Improve wayfinding to downtown.
3. Reinforce pedestrian activity.
4. Minimize conflicts between vehicles and pedestrians.
5. Provide enhanced facilities for festivals.
6. Develop a cohesive and unified character which respects the history of Cleveland within a progressive context.
7. Target new improvements to strengthen and unify visual character of the downtown.
8. Create a strong vibrant economic core, with easy pedestrian accessibility.



ASSESSMENT



WAYFINDING

Historically the downtown was developed around north-south traffic route. Over the years the historic downtown has been isolated from the the development of the interstate and by-passes which divert traffic around the city. The historic downtown has become an island, surrounded by high volumes of traffic and irregular strip related growth.

STRUCTURE

The historic nine square block downtown core is focused on the Bradley County Courthouse and bordered by Central Avenue to the north, Inman Street to the south, Church Street to the east and Worth Street to the west. Historically, Broad Street was the major street in the downtown area. For the most part, the historic area of downtown has remained intact, however the development of Inman Street into a major traffic artery for commuters has pulled the focus of the downtown one block south.

LAND USE

The historic core of downtown has remained intact. Lee University has developed to the north, manufacturing plants have developed predominantly to the south and southeast, residential development has occurred in the northwest quadrant of the city and is linked to the historic downtown and Lee University. Commercial development is spreading outward from the historic downtown along the major traffic routes.

ASSESSMENT



WAYFINDING

The downtown is difficult for visitors to locate from the by-passes or interstate.



PORTALS

There is a lack of definition to the boundaries of the historic downtown, and a lack of definition of arrival points.



PARKING

Existing parking resources are adequate, however they need to be better integrated with the pedestrian character of the historic downtown.



STREETSCAPES

The pedestrian character of the historic downtown needs to be reinforced through the development of a strong streetscape that promotes traffic calming and improved pedestrian safety.



COURTHOUSE SQUARE

This area has been reduced by the replacement of the historic courthouse with a much larger building. As a result there is very little open space and the area does not have a clear civic definition.



JOHNSTON PARK

The historic Johnston Park has lost its central focus because of the addition of multiple unrelated features. The park is inadequately sized for large festival events.



PARKER STREET / FIVE POINTS

Parker Street is a strong north-south connector into the historic downtown from the University. Currently there are emerging businesses in the Five Points Area and it has the potential for further expansion of residential, retail and parking.



MUSEUM AREA

The Museum Center/county building/memorial area lacks integration and unity, and is an area of unrealized potential. It is one of the most significant areas of downtown open space that is not being fully utilized.



OVERALL CHARACTER

The character of the historic downtown is interesting but can be strengthened through a consistent and coordinated use of site materials, site furniture and appropriate planting throughout the downtown.

COMMUNITY FORUMS

The community forums were a community driven process in which dialogue allowed city/county officials, business owners and residents to define what kind of place they would like their downtown to be in the future. Each individual and group involved brought a wealth of community knowledge, experience, and perspectives to the process. In the course of this Master Planning process, members of the public and private sectors as well as various interests groups in the community reached consensus on a number of items, while also identifying areas of disagreement, which then had to be resolved through compromise or creative solutions.

Mainstreet Cleveland employed a variety of means to reach out to the community at large for their involvement and input. They worked hard to inform their fellow citizens of the work being produced; even more importantly, they listened and learned from the people that call Cleveland their home. Mainstreet Cleveland conducted three public Community Forums, occurring at various stages of the Master Planning process to insure that the citizens of Cleveland were involved from conception of the Master Plan, through to its completion. During each Community Forum the public was invited to participate and to help create a combined vision for the future of downtown Cleveland. It was hoped that by choosing this grassroots approach the entire town will assume ownership of the plan and become actively involved in its implementation over the years to come.



1ST COMMUNITY FORUM

The first public forum was held on July 24, 2003, and was attended by representatives of Mainstreet Cleveland, Ross/Fowler, planning consultants and approximately 150 citizens of Cleveland. The meeting began with an introduction from representatives of Mainstreet Cleveland and Ross/Fowler, who explained the sequence of events needed to prepare a Master Plan and highlighted some of the trends, issues, and opportunities that Cleveland now faces; the potential for accelerated residential growth; the amount, type, and purposes of different kinds of open space in the community; and issues and opportunities facing the downtown.

After the opening presentation, the attendees were divided into eight discussion groups. Each group was handed a list of key discussion points and asked to talk amongst themselves; draw examples; and list ideas of what they thought downtown Cleveland's Master Plan should be. More specifically they were asked to react to the following topics: streets, parks and plazas, linkages and new development. At the end of the work period each group was asked to present its ideas to the rest of the groups, from which a collective list of ideas and concerns was prepared to serve as the conceptual basis for the master plan.



2ND COMMUNITY FORUM

After the first community forum, Ross/Fowler, and their team of consultants, worked to translate the input and the information collected into a vision and goals statement to guide the preparation of the conceptual Master Plan.

The second community forum on August 28, 2003 began with a presentation by Ross/Fowler who outlined their findings and initial recommendations to a group that included representatives from Mainstreet Cleveland, town officials, consultants and citizens of Cleveland. Ross/Fowler, together with their consultants, outlined the principles from which they had developed their ideas and presented the group with a series of historic images that summarized what they believed made small towns like Cleveland great. The discussion focused on areas of Cleveland that Ross/Fowler believed the influences of a Master Plan would have the greatest impact, including improvements to the courthouse and museum areas, redesign of the park and adjacent areas of 1st Street. At this time the conceptual Master Plan was introduced to the group, including initial concept plans for the areas that had been identified as high priority for re-development. The presentation was followed by an opportunity for feedback from the participants and attendees. Most people present agreed that the vision and goals included in the initial Master Plan accurately captured the Town's wishes for the future and was an appropriate platform from which to move ahead with the Master Plans.

3RD COMMUNITY FORUM

After the second community forum, follow-up meetings were held with each key stakeholder groups to further refine the main project areas before beginning the final stage of the Master Planning process. Ross/Fowler met with the city and county regarding the courthouse square, the museum board regarding the museum park/plaza and Maytag regarding property issues. From these meetings came re-designs to all concepts and further details were added, such as the addition of a band stand at the courthouse and the removal of the clock tower idea. After meeting with the museum board and Maytag, more parking and phasing of the park was developed.

The 3rd public forum was held on February 17, 2004. All plans and proposals were displayed for review and examination by the approximately two hundred participants. The final Master Plan was presented to all the attendees by Ross/Fowler, along with detailed plans of selected areas of Cleveland's downtown, the wayfinding Master Plan; and the design guidelines.

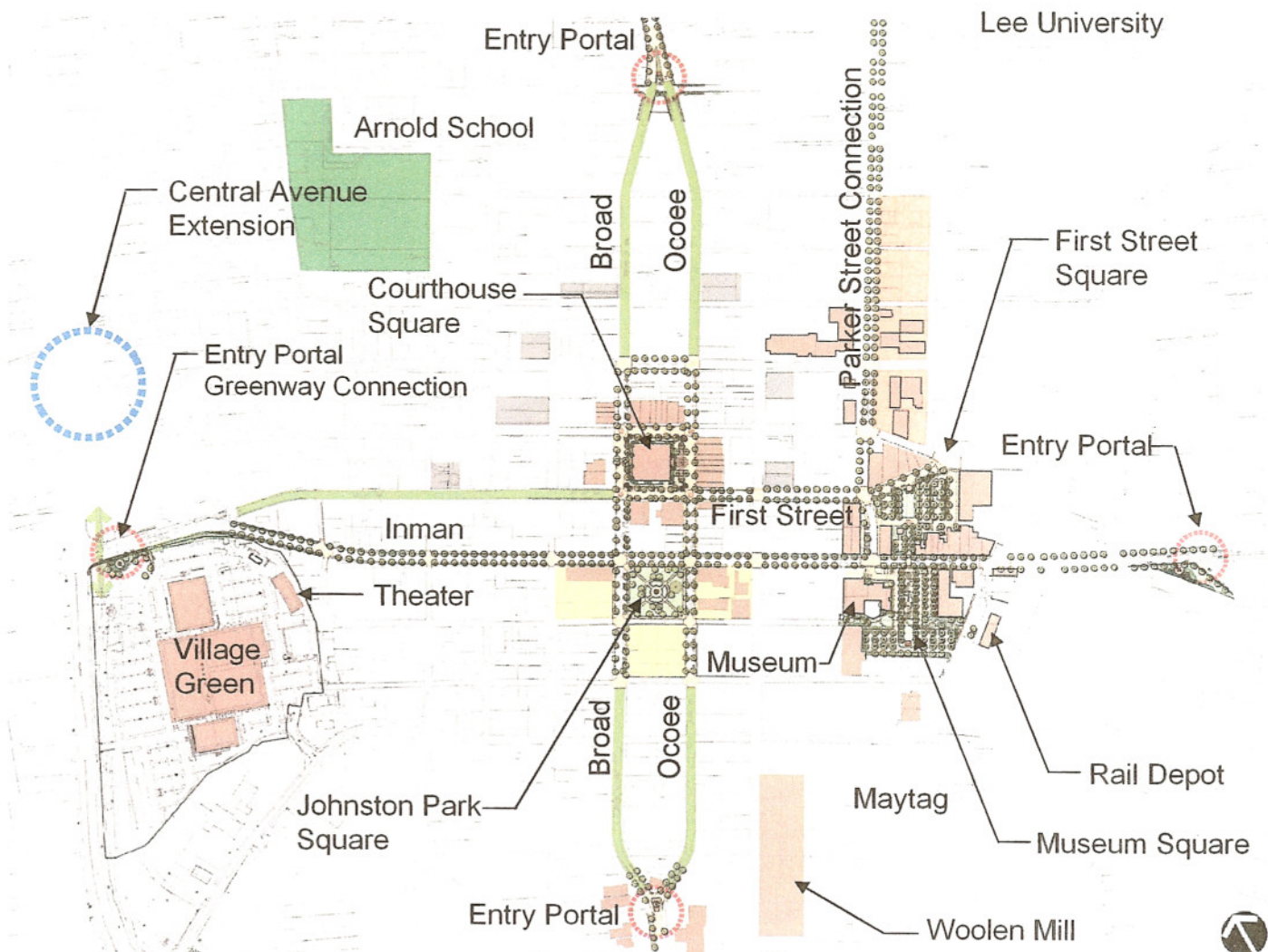


MASTER PLAN

CONCEPTS

The concept for the downtown Cleveland Master Plan is based upon the information gained from the Community Forums and from discussions with community leaders. The plan identifies four squares as improvement areas that are critical to Downtown – Courthouse Square, Johnston Park Square, First Street Square, and Museum Square. The influence of the Village Green and Lee University are noted as significant. Linkages in the form of pedestrian focused streetscapes are proposed to link the improvement areas, the Village Green, and Lee University while providing an upgraded uniform visual image for Downtown.

- **FOUR IMPROVEMENT AREAS**
 - Courthouse Square
 - Johnston Park Square
 - First Street Square
 - Museum Square
- **IMPROVED STREETSCAPES FOR BETTER PEDESTRIAN ENVIRONMENT AND UNIFORM DOWNTOWN IMAGE**
 - Inman Street
 - First Street North
 - Broad Street
 - Ocoee Street
 - Parker Street
- **IMPROVED PEDESTRIAN LINKAGE TO THE VILLAGE GREEN AND GREENWAY**
- **DEFINITION OF THE LIMITS OF DOWNTOWN THROUGH THE CREATION OF FOUR ENTRY PORTALS**
- **IMPROVED PEDESTRIAN LINKAGE ALONG PARKER STREET TO LEE UNIVERSITY WITH FUTURE RESIDENTIAL DEVELOPMENT POTENTIAL**
- **NORTH/SOUTH LINKAGE BETWEEN THE FIRST STREET SQUARE AND THE MUSEUM SQUARE TO CREATE A DESTINATION ATTRACTION AS WELL AS A LARGE FESTIVAL SPACE.**



MASTER PLAN

RECOMMENDATIONS

▪ **Wayfinding**

A system of wayfinding signage should be developed to direct travelers from the interstate and bypass into Downtown.

▪ **Portals**

Four Entry Portals should be developed to announce arrival into Downtown.

▪ **Streetscapes**

The streetscapes of the five key streets – Broad, Ocoee, Parker, First, Inman – should be improved with curb extensions, sidewalks, street trees, and street furniture.

▪ **Johnston Park Square**

Johnston Park should be simplified and returned to its original historic design. The adjoining/surrounding blocks should receive infill development.

▪ **Cherokee Hotel**

The Cherokee Hotel property should be investigated for use as a hotel/inn.

▪ **Museum Square**

The area south and east of the museum should be developed into a square with a destination attraction interactive water feature and interpretative plaza.

▪ **County Building**

The county building currently in use as an emergency response facility should be considered for a use more compatible and contributing to the Museum Square.

▪ **First Street Square**

The area southeast of the Parker/First Street intersection should be developed into a square with a central open space and parking.

▪ **Old Laundry Building**

The old laundry building currently in disuse should be acquired to allow for the development of the First Street Square.

▪ **Parker Street Corridor**

The Parker Street Corridor should receive pedestrian improvements and housing opportunities should be explored.

▪ **Housing**

Housing opportunities should be expanded in Downtown by the utilization of upper floors of existing buildings and the development of new residential structures as infill opportunities.

▪ **Courthouse Square**

The Bradley County Courthouse property and the immediate adjacent streets should be developed into a square of civic importance.

▪ **Village Green**

The Village Green development should be recognized as an important part of Downtown. Streetscape and pedestrian improvements should link the Village Green to Downtown.

▪ **Inman Street**

Inman Street and its' railroad underpass form a critical arterial for Downtown. Visually blighted West Inman and the entire length of Inman should be improved with safe sidewalks and street trees.

▪ **Traffic Study**

A rigorous detailed traffic study of Downtown should be undertaken to investigate the issues along Inman; the Central Avenue extension; and other issues of traffic concern in Downtown. The focus of the study should be to improve the pedestrian environment of downtown streets while maintaining appropriate traffic flow.

▪ **Lee University**

The University should be recognized as an important part of Downtown. Efforts should continue to seek development initiatives in Downtown that could be responsive to the student population.

▪ **Design Guidelines**

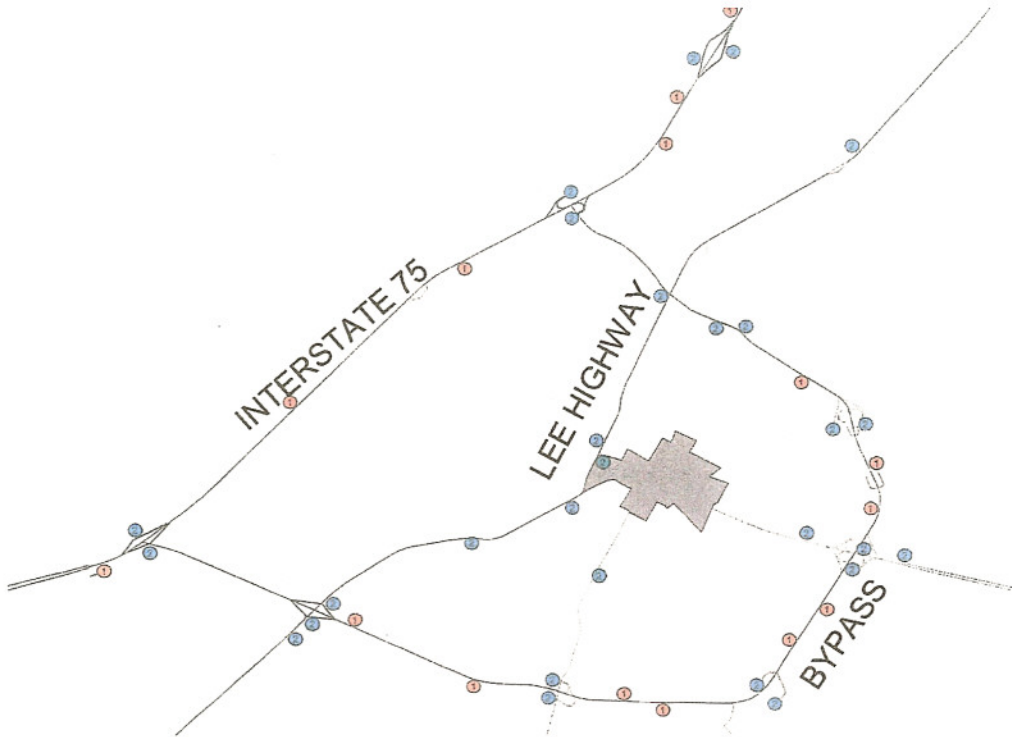
Coordinated street furniture, lighting and street trees should be used throughout Downtown as a unifying visual element.

▪ **Railroad Underpass**

The concrete walls of the railroad underpass on east Inman should be painted and highlighted in a pattern of architectural rusticated joints to add a historic design character to the underpass. The warning signs and lights should be evaluated to better integrate with the structure.

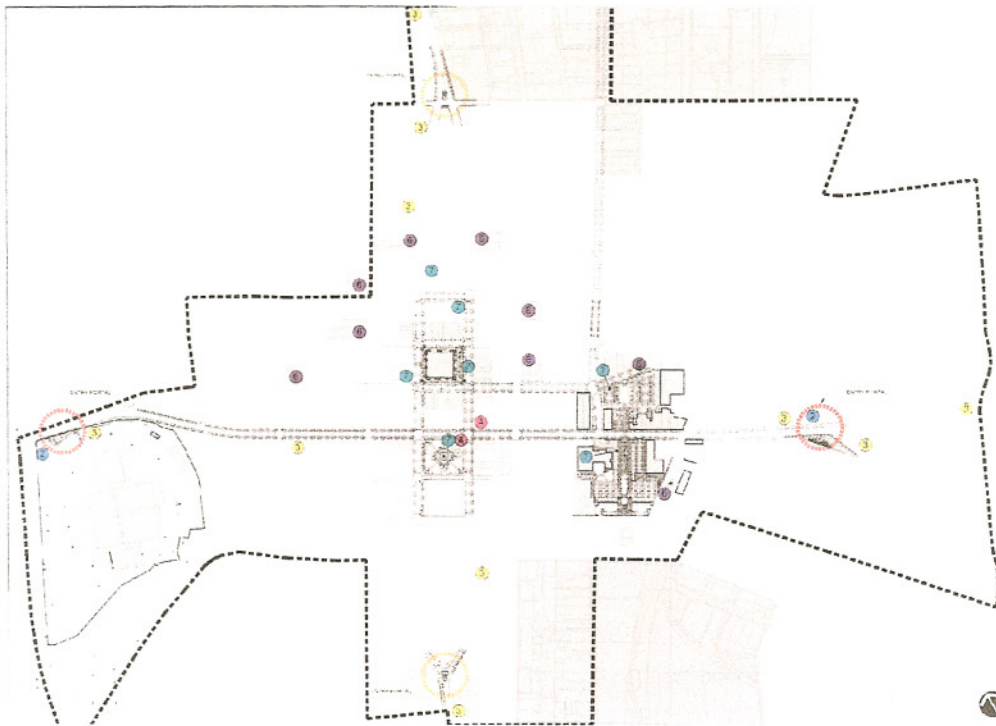
WAYFINDING

A coordinated signage system has been developed to assist motorists in navigating from the interstates and by-passes surrounding the town of Cleveland, into the Historic Downtown core.



PERIMETER SIGNAGE

Perimeter signs are intended to direct the motorists into the historic downtown. Signs should be designed to be read from a distance and at the design speed of the roadway. They shall be located at the main roads traveling in and out of the city and especially at key intersections.



CORE SIGNAGE

Core signage is designed to direct the motorist and pedestrian to key downtown sites such as the Museum, the Courthouse, the library, parking, etc. These signs are located in advance of any decision point and will visually correlate with perimeter signage.

WAYFINDING



1 INTERSTATE ATTRACTION SIGN

The interstate signs is to be located upstream of key interstate ramps to allow motorists sufficient time to make decisions to travel into historic downtown.



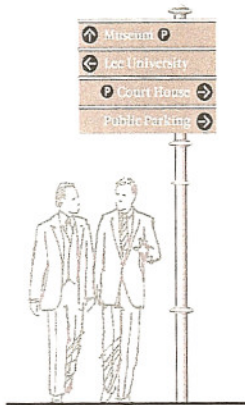
2 VEHICULAR GUIDE SIGN

Vehicular scaled sign at key points off the interstate which direct traffic to the historic downtown.



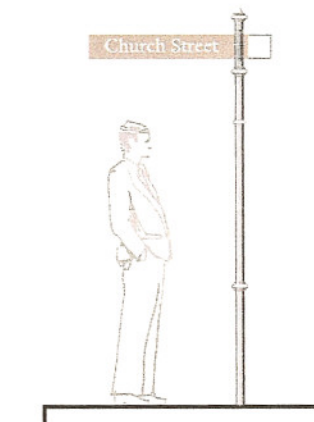
3 VEHICULAR GUIDE SIGN

Stylized vehicular signage outside the boundaries of the downtown, that relate more to the scale and historic nature of the downtown.



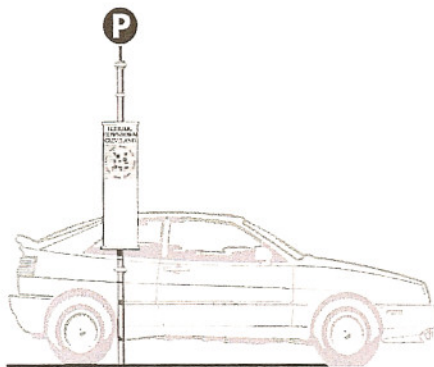
4 VEHICULAR DIRECTION SIGN

Once you have arrived into the historic downtown, these stylized signs provide direction to specific points and key places within the downtown area.



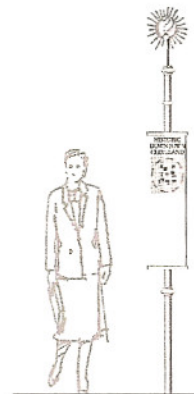
5 STREET NAME SIGN

Street signs intended to reinforce the historic nature and limits of the historic downtown.



6 VEHICULAR PARKING SIGN

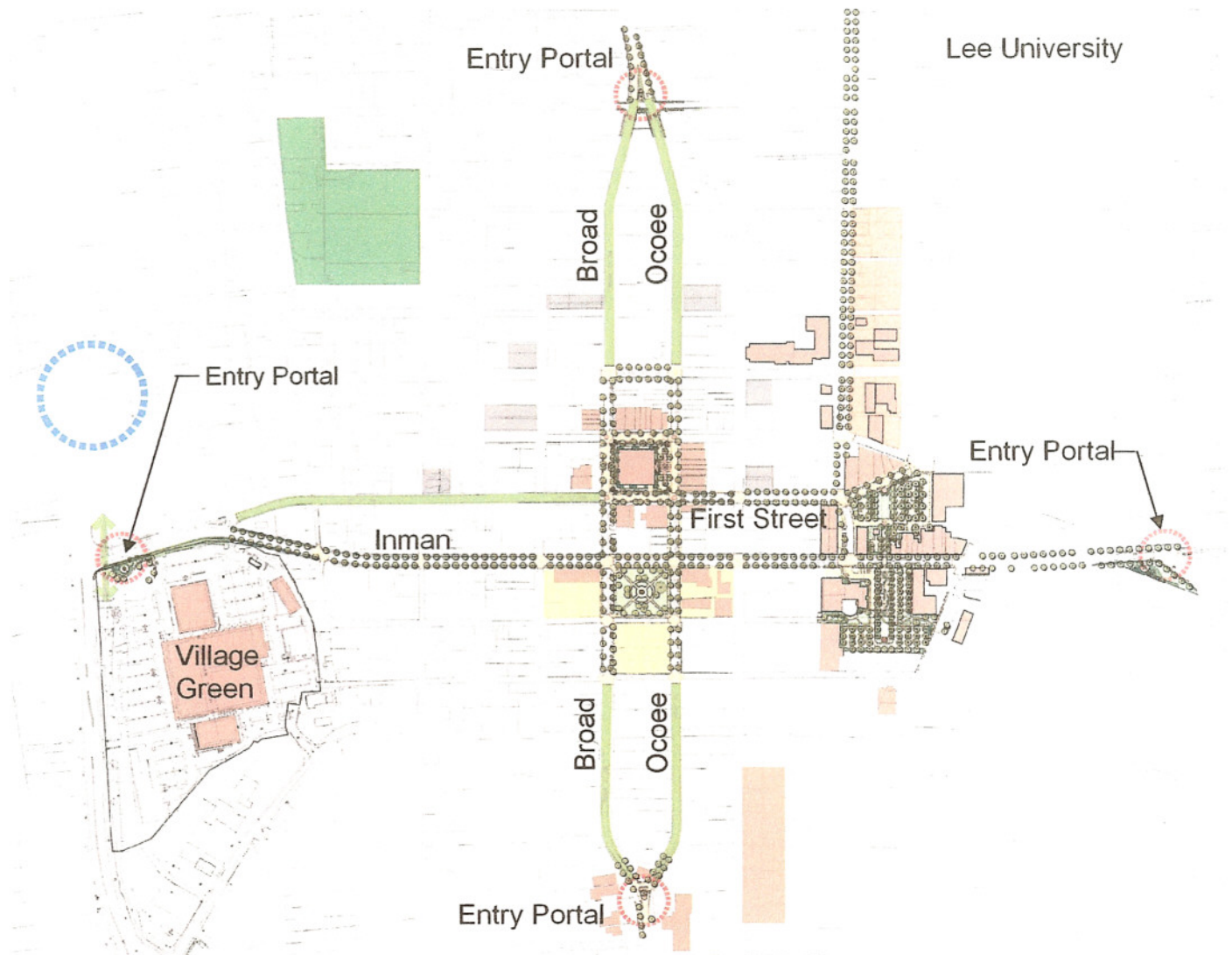
Stylized parking marker at specific parking locations that are intended to direct motorists into downtown Cleveland once they have left their vehicles and become pedestrians.



7 PEDESTRIAN INFORMATION / DIRECTION SIGN

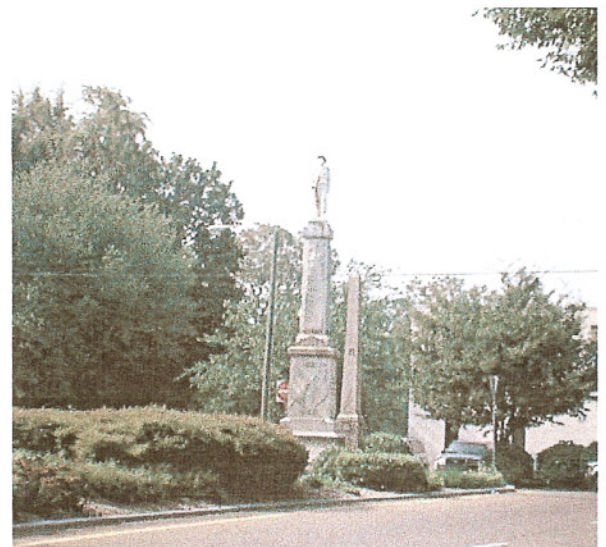
Stylized pedestrian related marker which provides a further breakdown of downtown events and specific destination points within the historic downtown.

PORTALS

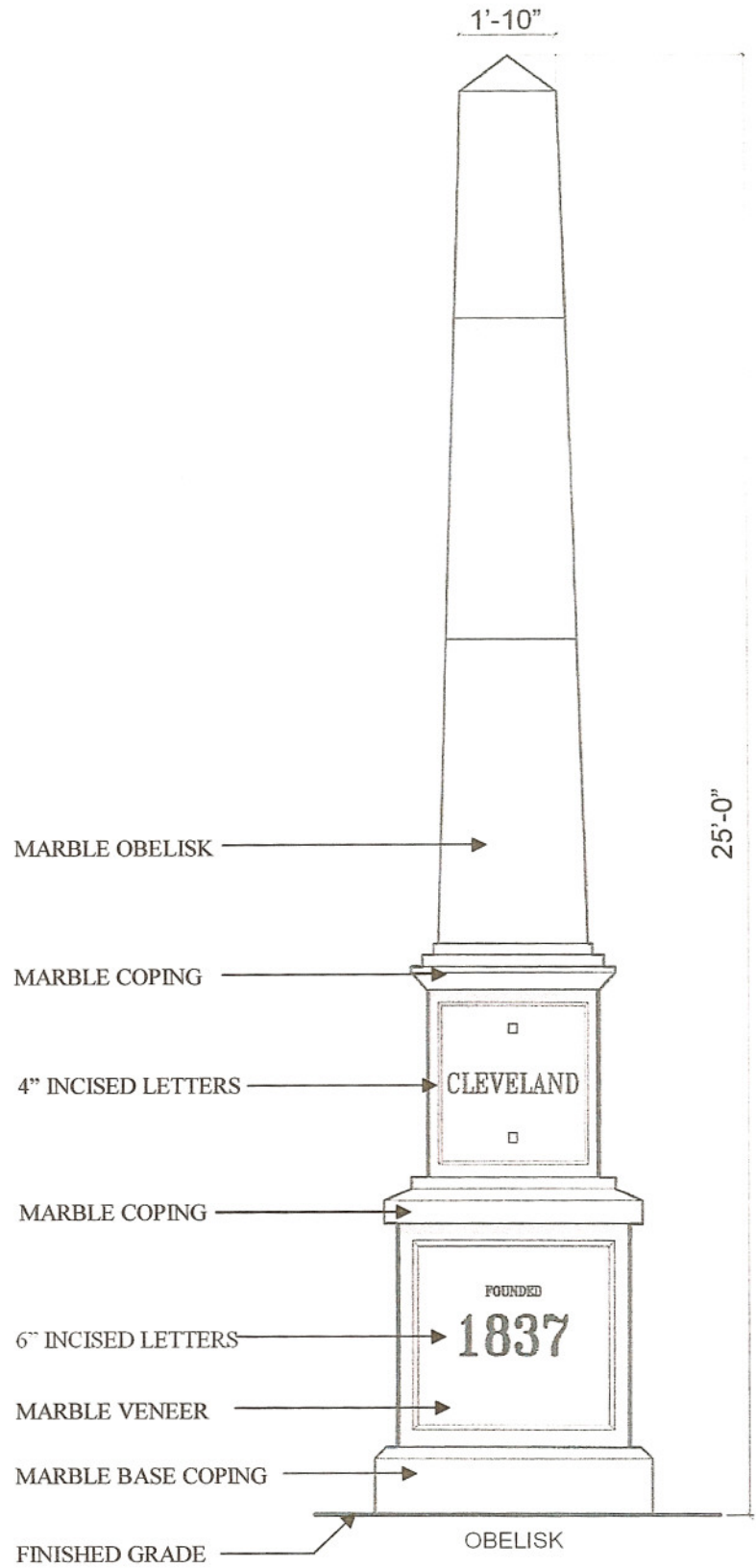


Entry portals should be constructed to further define the sense of arrival into historic downtown Cleveland and to build upon the image of the historic monuments located near Eighth, Broad, and Ocoee street. Four entry portals are required and they are located at Inman Street & 1st Street NW, Broad Street NW & North Ocoee Street, Broad Street SW & Ocoee Street, and Inman Street & Wildwood Avenue SE.

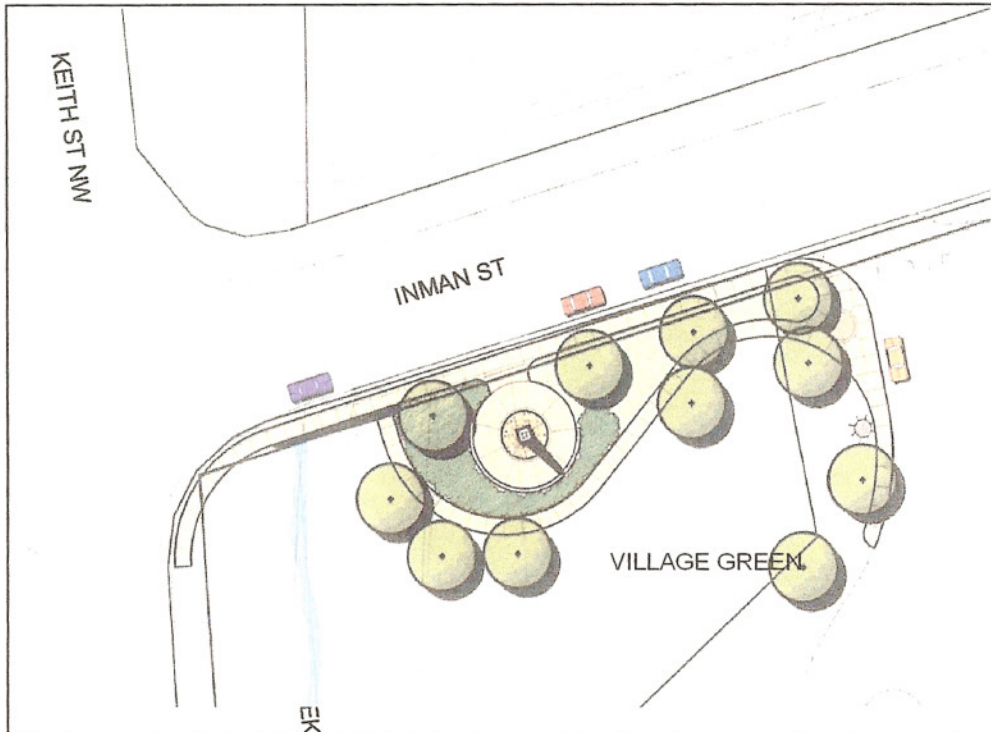
Each entry portal should utilize an obelisk fashioned after the existing obelisk located at Broad Street NW & North Ocoee Street. Walkways, seasonal color plantings, seat walls and lighting should be among the elements used to reinforce the entry portal design and lead the visitor into historic downtown Cleveland.



PORTALS

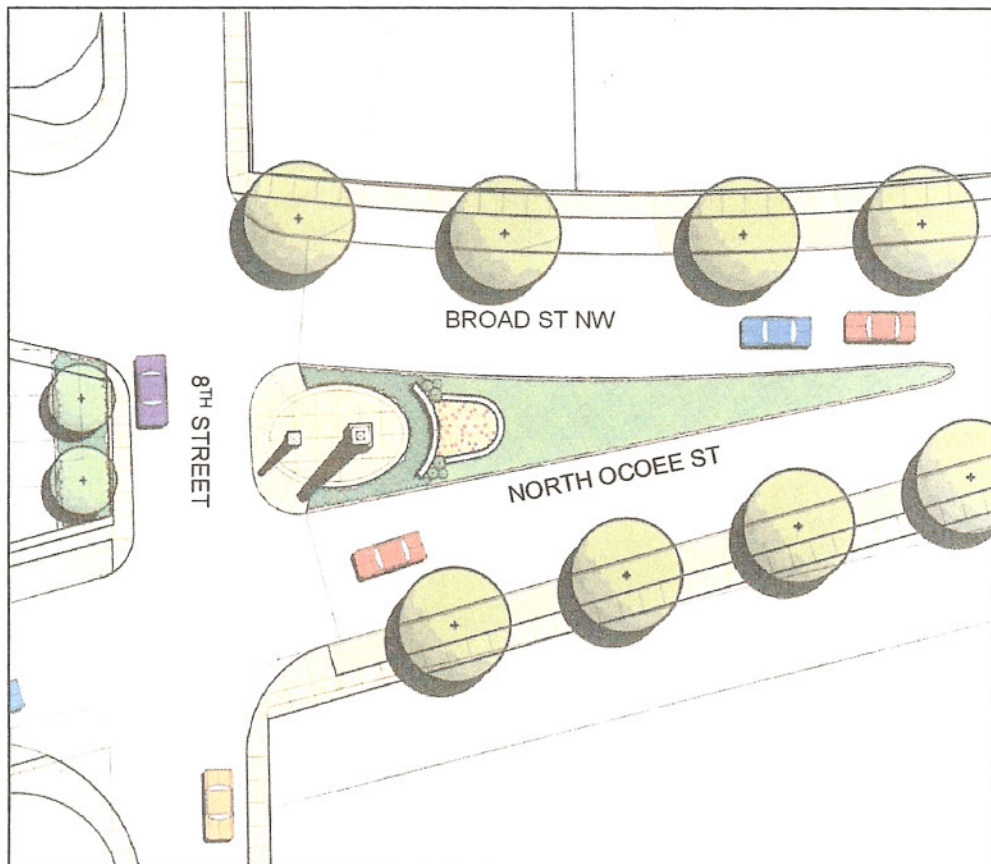


PORTALS



INMAN STREET & KEITH STREET

The west entry portal into downtown at Inman and 1st Street shall have a walkway around the perimeter that will eventually be linked to the existing greenway. A seasonal color bed will be added at the base of the obelisk for year round interest and a seat wall will encompass the focal point to serve as a place to gather.



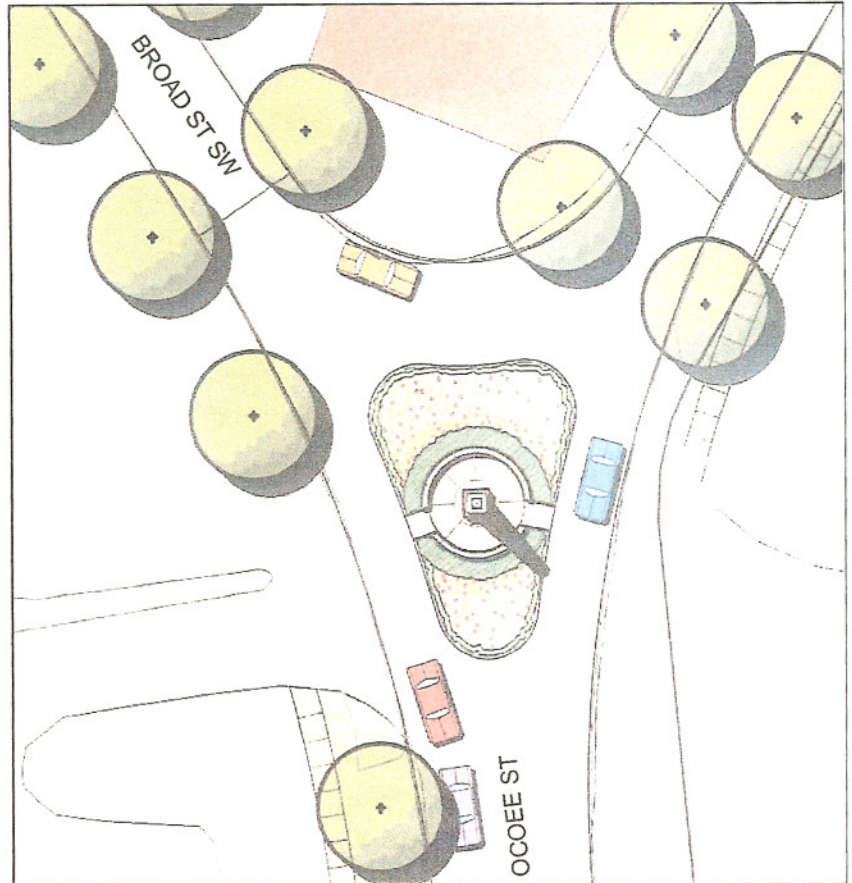
BROAD STREET & NORTH OCOEE STREET

The north entry portal into downtown at Broad Street and north Ocoee Street contains two existing monuments that shall be preserved in place. The future design of entry portal obelisks shall respect and mimic the design shown on the smaller monument.

Planting and site features have been added to enhance the current entry to include: seasonal color, street trees, lawn, a sign, and paving. The unity of these elements creates an appealing invitation into the core of historic downtown Cleveland.

BROAD STREET SW & OCOEE STREET

The south entry portal into downtown at Broad Street SW and Ocoee Street consists of an island with the obelisk being the central focus. The circular paving is outlined by a seat wall that has an evergreen shrub backdrop. Seasonal color beds fill in the remaining area and add an attractive punctuation to this entry.



INMAN STREET & WILDWOOD AVENUE SE

The east entry portal at Inman Street and Wildwood Avenue SE contains the same unifying elements as shown on the previous three entry portals. The obelisk at this location has an evergreen backdrop to enhance its' presence. The paving connects to an existing walkway. The evergreen groundcover helps to create an interesting element and to further reinforce the design.

