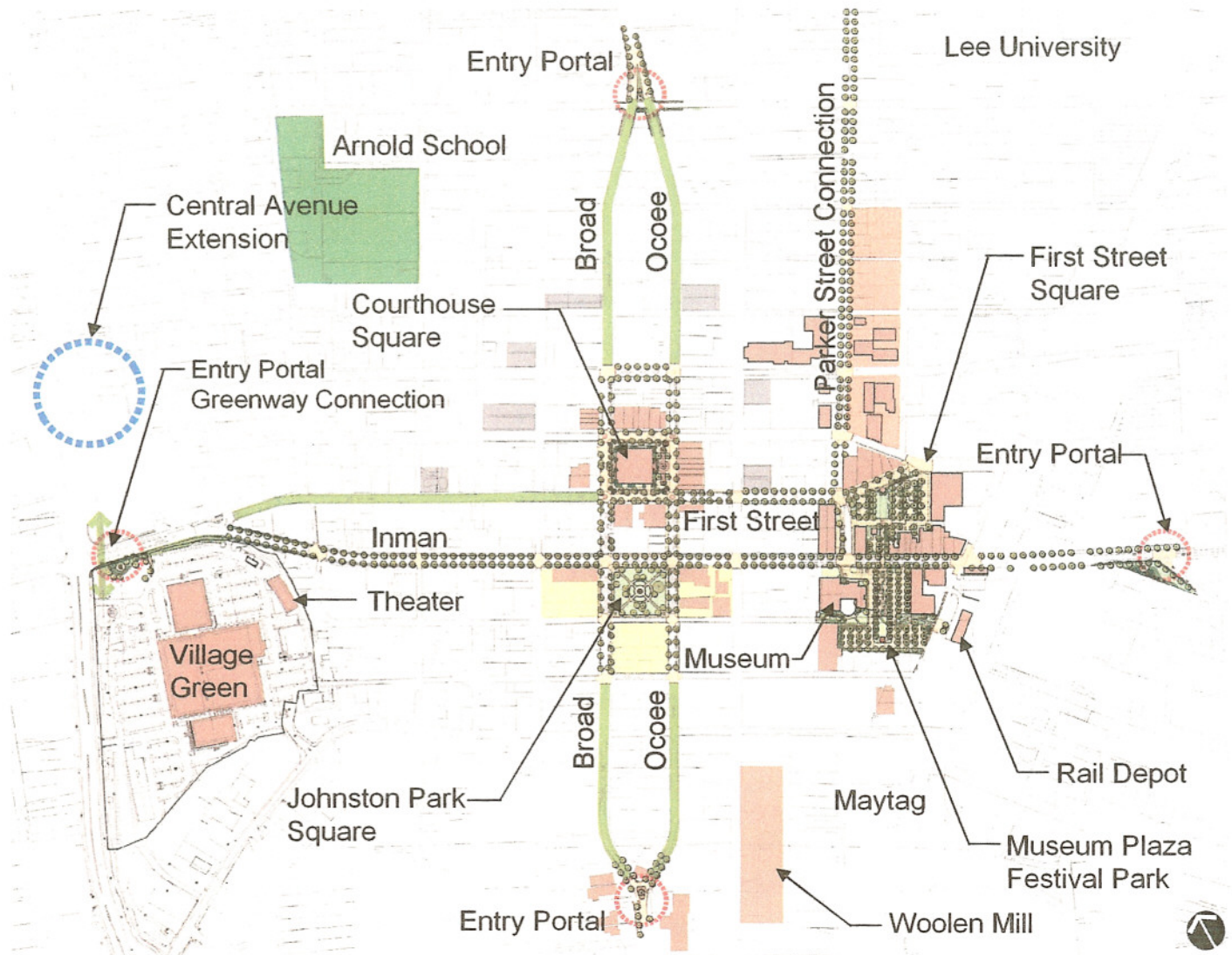


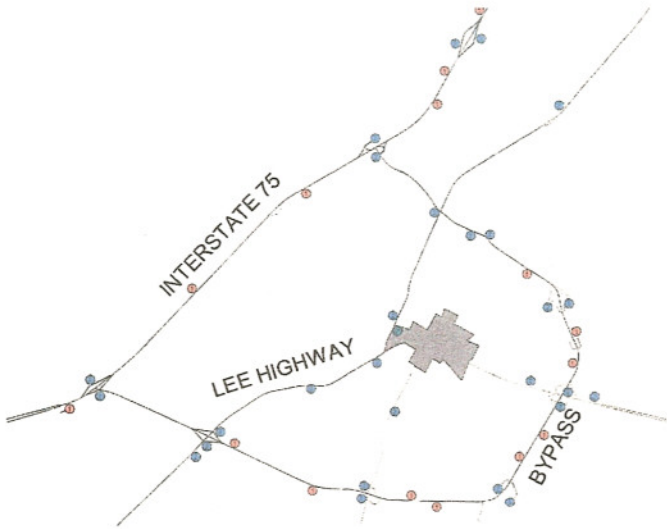
MASTER PLAN IMPLEMENTATION



Implementation of the Downtown Cleveland Master Plan will require public/private cooperation and a sustained commitment over a number of years. The focus of activities should be to establish and maintain a sense of forward momentum while adapting to changing conditions. Although implementation of the Master Plan is a complex process it can be distilled into several key components.

- **Facilitate plan adoption by local government and the business community.**
 - City
 - County
 - Chamber of Commerce
- **Establish a process of continuing advocacy for plan implementation.**
 - Project Champions
 - Annual action plans
- **Identify action items and priorities**
 - Schedule
 - Quick results
 - High impact
- **Determine critical path of implementation**
 - Public responsibilities
 - Private responsibilities
- **Investigate state and federal funding sources**
 - Congressional set aside
 - TEA 21
 - Urban Forestry Grants
- **Assess local funding mechanism**
 - Annual capital improvements plan
 - Tax increment financing
 - Central Business Improvements District
 - Private philanthropy
- **Investigate development incentives**
 - Tax abatement
 - Matching grants

MASTER PLAN IMPLEMENTATION



WAYFINDING

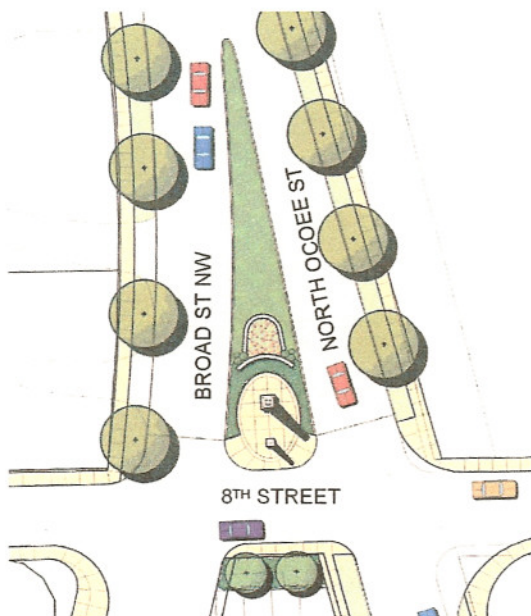
To initiate the implementation of the wayfinding Master Plan, the City of Cleveland must submit a proposal to the Tennessee Department of Transportation to seek their comments and approval. The plan can then be implemented through private contractors, the city sign shop, or a combination of both.

Key Participants:

City of Cleveland, Bradley County, Tennessee Department of Transportation

Funding :

City of Cleveland, Bradley County, Mainstreet Cleveland, Chamber of Commerce



ENTRY PORTALS

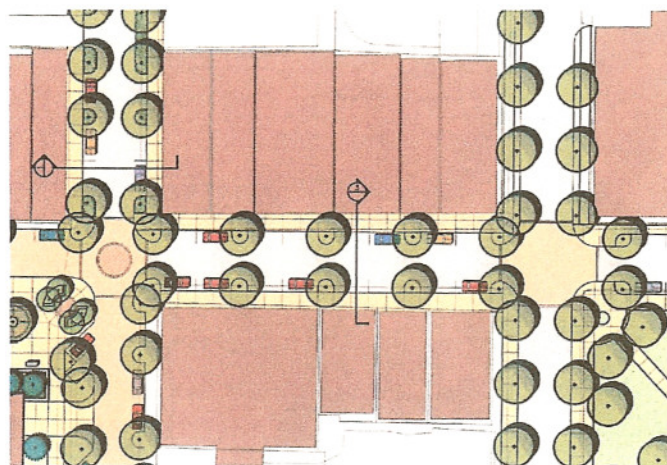
The implementation of the entry portals requires a partnership between public and private interest groups. These small scale projects will require careful and detailed design to assure quality/high impact arrival points.

Key Participants:

City of Cleveland, Mainstreet Cleveland, Private Property Owners

Funding :

City of Cleveland, Private Interest Groups, T21 Grants



STREETSCAPES

The implementation of streetscapes improvements requires a continuing commitment for the strengthening of the five key streets in the historic downtown. Tennessee Department of Transportation cooperation and approval will be required for the state routes of Inman St. and Broad St. A detailed traffic study is recommended for Inman to balance traffic flow, pedestrian safety and appearance issues.

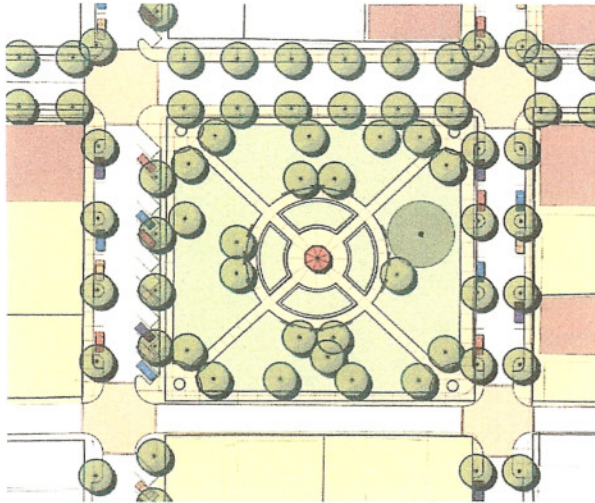
Key Participants:

City of Cleveland, Private Interest Groups, Tennessee Department of Transportation

Funding :

T21 Grants, City of Cleveland, Private Interest Groups

MASTER PLAN IMPLEMENTATION



JOHNSTON PARK SQUARE

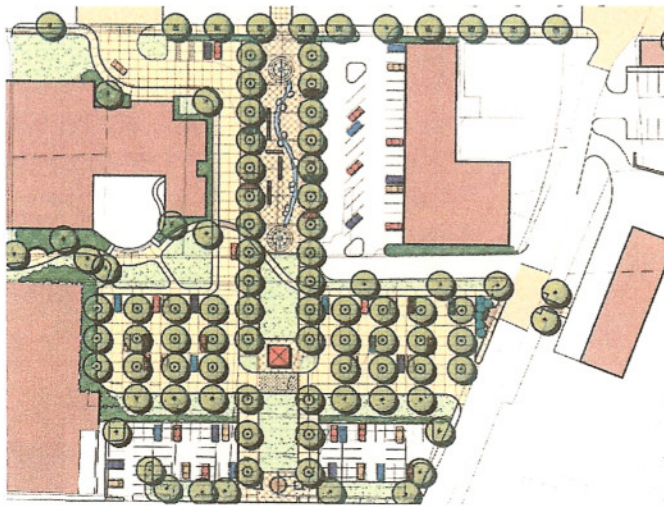
The implementation of the Johnston Park Square redevelopment requires a consortium of public and private interests. The private interests should focus on the redevelopment of the hotel and development of the surrounding infill opportunities. The City of Cleveland should focus on the redevelopment of the park to its original historical design.

Key Participants:

Private Developers, City of Cleveland

Funding :

Private Developers, City of Cleveland



MUSEUM PLAZA SQUARE

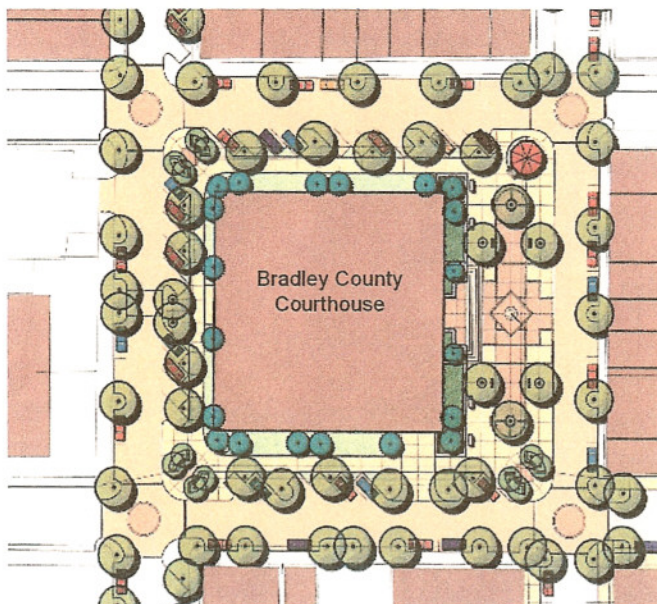
The implementation of the Museum Square requires a partnership between the Museum, City of Cleveland, Bradley County and surrounding property owners. Long term it is hoped that the County Building will be used for a contributing land-use for the Museum Square.

Key Participants:

Museum, City of Cleveland, Bradley County, surrounding property owners

Funding :

Private Interest Groups, City of Cleveland



COURTHOUSE SQUARE

The implementation of the Courthouse Square requires partnership between the City of Cleveland, Bradley County, Tennessee Department of Transportation, surrounding property owners and other private contributors. This project could offer a prime example of public and private cooperation in historic downtown Cleveland. Detailed, context sensitive designs of these important public spaces is critical to the success of this centrally located civic project in the historic downtown.

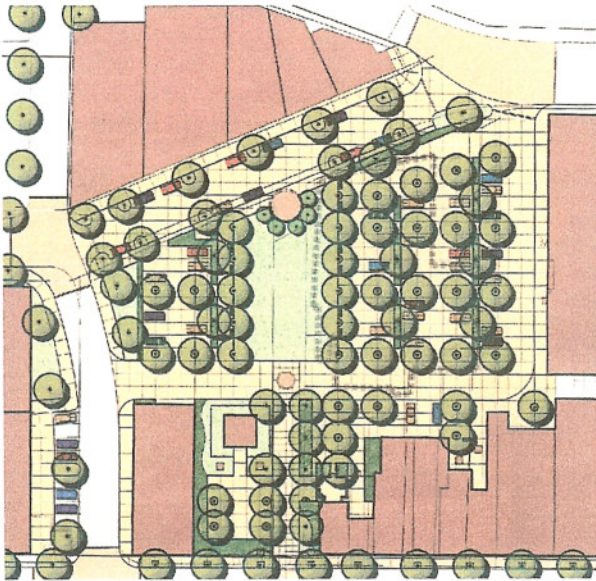
Key Participants:

City of Cleveland, Bradley County

Funding :

City of Cleveland, Private Interest Groups, T21 Grants

MASTER PLAN IMPLEMENTATION



FIRST STREET SQUARE

The implementation of First Street Square is dependant upon the acquisition of the old laundry building and land which is located in the center of the proposed square, as well as the resolution of alley access issues between the City of Cleveland and private Property owners.

Key Participants:

Private Owners, City of Cleveland

Funding :

Private Interest Groups, City of Cleveland



FIRST STREET/PARKER STREET

The implementation of the First Street and Parker Street development requires a partnership between Lee University, First Baptist Church and other private property owners, in the development of an appropriate mix of infill housing and Church parking while strengthening the pedestrian nature of Parker Street.

Key Participants:

First Baptist Church, City of Cleveland, Lee University, Private Property Owners

Funding :

City of Cleveland, Lee University, Private Interest Groups



DESIGN GUIDELINES

The implementation of the Design Guidelines will require consistent application of the guidelines throughout the historic downtown for a sustained period of time.

Key Participants:

City of Cleveland, Mainstreet Cleveland

Funding :

City of Cleveland, Mainstreet Cleveland

DESIGN GUIDELINES

BENCHES

Timberform Renaissance bench with armrests
Manufacturer: Columbia Cascade
Product Number: 2806-6
Color: Black
Contact: (800) 547-1940

Application

The Renaissance bench shall be positioned at appropriate locations throughout downtown for directional seating to enhance outdoor interaction and contemplation.



SEATS

Timberform Renaissance seat with armrests
Manufacturer: Columbia Cascade
Product Number: 2802-6
Color: Black
Contact: (800) 547-1940

Application

The Renaissance seat shall be positioned at appropriate locations throughout downtown for multi-directional seating to enhance outdoor interaction and contemplation.



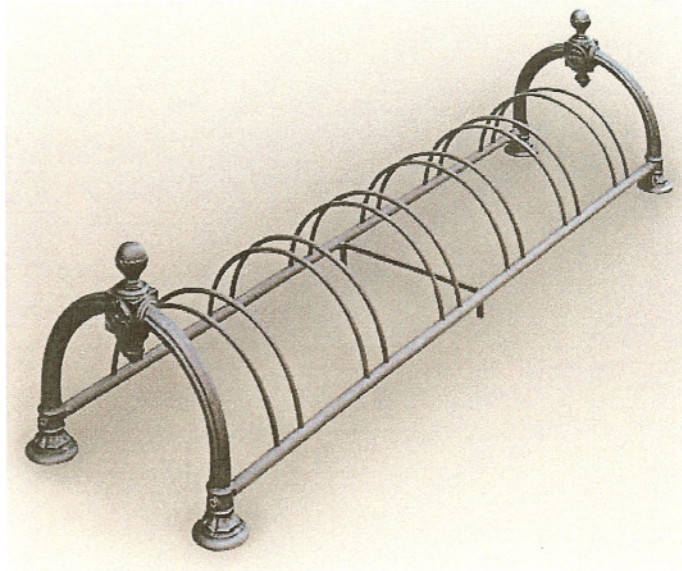
DESIGN GUIDELINES

BIKE RACKS

Manufacturer: Canterbury International
Model: Milano Bike Rack
Color: Black
Contact: (800) 935-7111

Application

Secure bicycle parking shall be provided in convenient but visually discrete locations.



PLANTERS

Manufacturer: Canterbury International
Model: Pennsylvania Avenue Planter
Color: Black
Contact: (800) 935-7111

Application

The planter shall be used to define building entries and to direct both pedestrian and vehicular movement. These planters shall be installed and planted with appropriate seasonal color.



DESIGN GUIDELINES

LITTER RECEPTACLES

Manufacturer: Canterbury International
Product Number: Pennsylvania Avenue Trash Receptacle
/ Model 101
Color: Black
Contact: (800) 935-7111

Note:

Method of attachment shall be either of the following:

1. Direct embedment with footing sized per manufacturer's recommendations.
2. Attached to pavement with expansion anchor per manufacturer's recommendations.

Application

Their placement shall follow logical gathering spots near building entries, outdoor seating areas, intersections of pathways, outdoor recreation areas and along pedestrian ways.



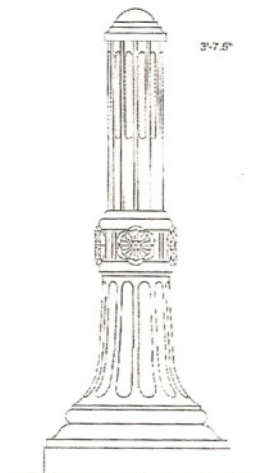
BOLLARDS

Manufacturer: Holophane
Model Number: - BOL/CH44/12/DT/L-CA/BK
- BOL/CH44/12/DT-CA/BK*
(*NON-ILLUMINATED)

Color: Black
Lamp type: HIGH PRESSURE SODIUM
Contact: (614) 882-7243

Application

The bollard light shall be used in a pedestrian application where the pole light fixture is not appropriate.



DESIGN GUIDELINES

POLE LIGHT FIXTURES

Application

The single pole light fixture shall be used for pedestrian and vehicular situations to include but not limited to parking lots, plazas, open spaces, and street lighting. The double drop acorn pole light should be used in very limited areas and should be reduced in height to be more compatible to the surrounding historic structures.

Manufacturer: Holophane
Model Number: GVU100HP12B3RSG-H-F1
Post Model Number: NY15/17FRC-FT/BK-DDBB-RS/GFI/WPC
Color: Black
Lamp type: High Pressure Sodium
Contact: (614) 882-7243



Manufacturer: Holophane
Model Number: - CR250HP12B4-PR-PS
Post Model Number: AT30FF12/S/BK-OL51-22CSB-BK-(2) BB36A/1/CO-RS/GFI/WPC
Color: Black
Lamp type: HIGH PRESSURE SODIUM
Contact: (614) 882-7243



DESIGN GUIDELINES

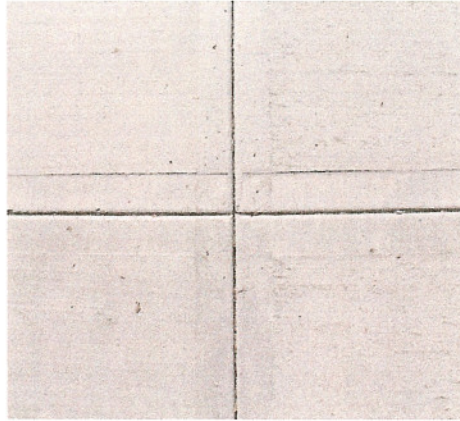
CONCRETE

Concrete - Should be the standard pedestrian walking material used throughout downtown.

- Concrete shall be 4" thick natural grey with reinforcing. All joints shall be 3/8".
- Edges and joints shall have 3" wide smooth trowel edges.
- Finish shall be a heavy raked finish, perpendicular to the longitudinal axis.
- Score joints shall be 10' o.c. maximum.
- Expansion joints shall be 40' o.c. maximum and shall be constructed with smooth internal dowels at the joints to prevent vertical movement.
- All concrete paving shall be sloped to drain at 1% minimum.
- Material and design shall be ADA compliant

Application

Applications for concrete shall be sidewalks, plazas, accent paving, banding and parking lot accent paving.



BRICK

- Brick paver system shall consist of pavers on a reinforced concrete slab with a 1" thick mortar bed. Herringbone, Running Bond, and Basketweave are all acceptable brick patterns.
- Brick paving shall be constructed with tight joints.
- Expansion joints shall be as required with an expandable joint compound with color to match joints.
- Brick paver color shall match surrounding building architectural color.
- Brick pavers shall be a standard 4" x 8" rectilinear shape. Paver thickness shall be 2" minimum.
- Material and design shall be ADA compliant

Application

Applications for brick paving shall be accent paving areas and banding. This material shall be reserved for high impact and important pedestrian gathering areas.



DESIGN GUIDELINES

CONCRETE PAVERS

- Concrete paver system shall consist of a concrete paver on a reinforced concrete subbase with a mortar bed.
- The paver shall be minimum 2 3/8" thick.
- Paving pattern shall be Appian Stone or equal with 5 1/2 x 5 1/2 sq. unit and a 5 1/2 x 8 1/4 rectangular unit available from Paver Systems.
- Color shall be a granite blend.
- Unit pavers shall have 1/8" swept joints butted together.
- Material and design shall be ADA compliant

Application

Applications for concrete pavers shall be pedestrian accent paving areas and banding. This material shall be reserved for high impact and important pedestrian gathering areas.



VEHICULAR PAVING

- Street print paving pattern shall be Offset Brick
- Color shall be Brick
- Material and design shall be ADA compliant
- Contact: (888) 581-2299

Application

Applications for special vehicular paving shall be reserved for special streets, intersections, and crosswalks.



DESIGN GUIDELINES

INTERSTATE ATTRACTION SIGN

- Panel:** Aluminum clad phenolic impregnated honey-comb panels. Panel face to have 3M Diamond Grade sheeting with Electrocut translucent film overlay, and graphics and / or legend reverse cut from overlay. Logotype shall be Galliard Regular and lower legend shall be Highway Gothic. Face graphics to conform to MUTCD standards.
- Post:** "I" column vertical supports with standard "break-away" bases, into engineered concrete footings. Column / panel connections to be square head bolt into integral channel at panel rear with Post Clip connection.
- Manufacturer:** Per City of Cleveland Standards

Application

This interstate attraction sign should be used in advance of the three exits leading to actual connections into Downtown.



DESIGN GUIDELINES

VEHICULAR GUIDE SIGN

Application

Locate along arterial streets and at decision points leading to Downtown.

Panel: Aluminum sheet panel. Panel face to have 3M Diamond Grade sheeting with Electrocut translucent film overlay, and graphics and / or legend reverse cut from overlay. Logotype shall be Galliard Regular and Arrowform shall be Project Standard Arrow (rotated as required). Face graphics to conform to MUTCD standards.

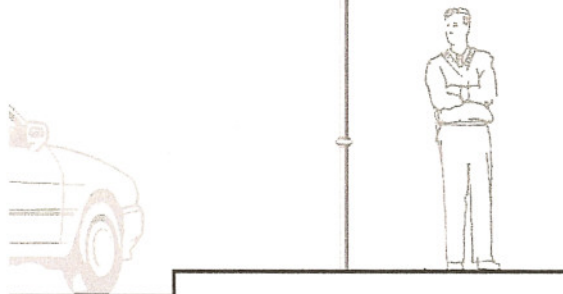
Manufacturer: Per City of Cleveland Standards



Post: Square-section column vertical support with standard 'break-away' bases, into engineered concrete footings. Column / panel connections to be mechanically bolted between sign panel and column 'tabs'.

Application

Locate along city streets in core leading to Downtown.



Post: Decorative column vertical support, into engineered concrete footings. Column / panel connections to be mechanically bolted between sign panel and column "tabs".

DESIGN GUIDELINES

VEHICULAR DIRECTIONAL SIGN

- Panel:** Aluminum sheet panel. Panel face to have 3M Diamond Grade sheeting with Electrocut translucent film overlay, and graphics and / or legend reverse cut from overlay. Legend shall be Galliard Regular and Arrowform shall be Project Standard Arrow (rotated as required). Pictograms shall be Project Standard Pictogram image in graphic disk as shown. Face graphics to conform to MUTCD standards.
- Post:** Decorative column vertical support, into engineered concrete footings. Column / panel connections to be mechanically bolted between sign panel and column "tabs".
- Manufacturer:** Per City of Cleveland Standards

Application

Application along city streets in the core of Downtown and at key decision points.

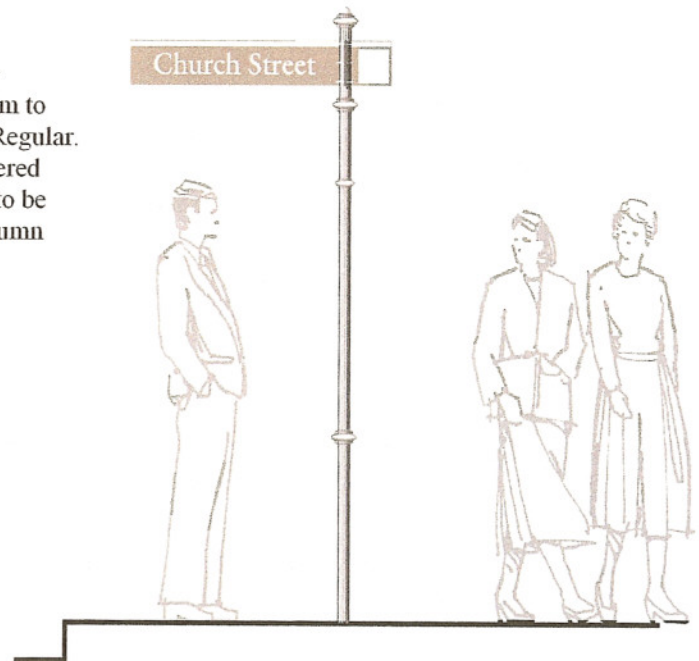


STREET NAME SIGN

- Panel:** Aluminum sheet panel. Face graphics to conform to MUTCD standards. Legends shall be Galliard Regular.
- Post:** Decorative column vertical support, into engineered concrete footings. Column / panel connections to be mechanically bolted between sign panel and column "tabs".
- Manufacturer:** Per City of Cleveland Standards

Application

Existing inventories of street signs across Downtown shall be replaced with the approved standard. Coordinate with City of Cleveland and TDOT as required.



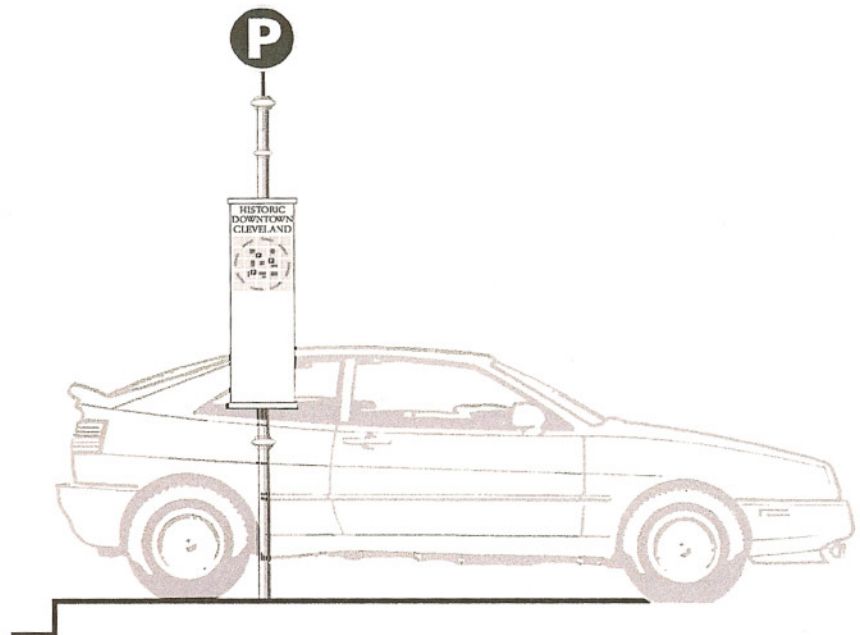
DESIGN GUIDELINES

VEHICULAR PARKING SIGN

- Panel: Map "box" to be painted fabricated aluminum cabinet with surface PSV graphics from e-files.
- Post: Decorative column vertical support, into engineered concrete footings. Column / panel connections to be mechanically bolted between sign panel and column "tabs".
- Manufacturer: Per City of Cleveland Standards

Application

Locate centrally or at entry to all public parking facilities.

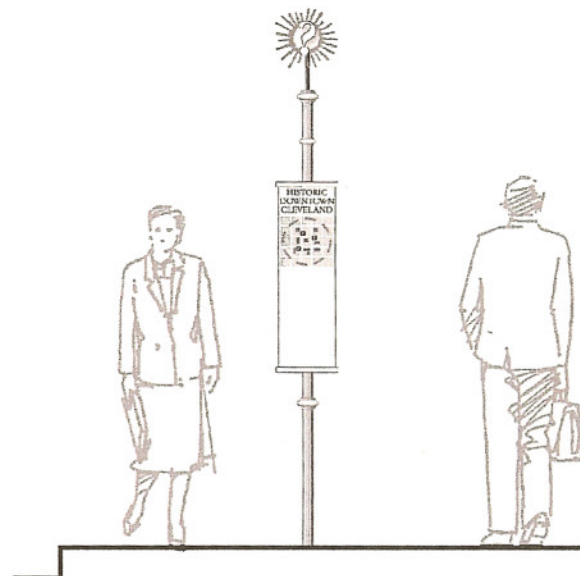


Top disk to be painted fabricated aluminum element, with surface PSV graphics.

PEDESTRIAN INFORMATION / DIRECTIONAL SIGN

Application

Locate in or near main pedestrian gathering areas or at key destinations.

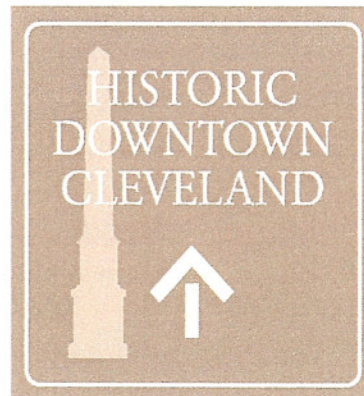


Top disk to be painted laser-cut-fabricated aluminum element.

DESIGN GUIDELINES

Three logo types were investigated for use in the Wayfinding signage. One refers to the nine square core of Downtown; another utilizes the obelisk image from the portals; the third image references the historic architecture of the Craig Miles structure.

ALTERNATE LOGO TYPES



DESIGN GUIDELINES

TREE LIST

The prototypical tree list seeks to establish the quality and character of plantings in Downtown Cleveland and is not meant to be an all inclusive list. The list is comprised of both native and adaptive species.

Minimum allowable Planting Sizes:

- Shade trees – 4” caliper
- Street trees – 4” caliper
- Flowering trees – 3” caliper
- Evergreen trees – shall be branched to ground with minimum height of 8’
- Screen trees – shall be branched to ground with minimum height of 8’

Street Trees

1. *Acer rubrum* – Red Maple
2. *Acer saccharum* – Sugar Maple
3. *Liquidambar styraciflua* ‘Rotundiloba’ – Sweetgum (fruitless)
4. *Platanus x acerifolia* – London Planetree
5. *Quercus nigra* – Water Oak
6. *Quercus nuttallii* – Nuttall Oak
7. *Quercus phellos* – Willow Oak
8. *Quercus rubra* – Red Oak
9. *Tilia cordata* – Littleleaf Linden
10. *Ulmus parvifolia* ‘Emer II’ – Allee Elm

SEASONAL COLOR BEDS

Due to the high maintenance costs of seasonal color beds, their use should be limited to high impact areas only. Those areas should include concentrated pedestrian and vehicular use areas as well as all portal entries.

The installation of seasonal color beds shall be in conformance with the following standards:

- The seasonal color plant list shall be rotated two times per year:
 - A. Late spring / early summer planting may include summer annuals such as begonias, marigolds, lantana, and impatiens.
 - B. Fall planting shall include the installation of winter pansies as well as spring blooming tulips.

Application

- Significant Intersections
- Portals
- High Pedestrian Locations
- Planters



DESIGN GUIDELINES

UNDERGROUND UTILITIES

The existing downtown condition contains many overhead utilities which add to the visual clutter. It is recommended that all possible overhead utility lines be relocated underground where possible.

- As future projects are developed, Cleveland should work with local utility companies to relocate above ground street light utilities below grade.

IRRIGATION

- All new planting improvements shall be irrigated with an automatic irrigation system.
- The irrigation system shall be designed to provide 100% irrigation coverage for all trees, shrubs, turf and seasonal color beds.
- Turf, shrubs and color beds shall be irrigated on separate systems.
- All systems shall be designed compatible with a future central control system.
- Irrigation systems shall be designed efficiently for water conservation and shall include rain sensors.
- It is recommended that existing non-irrigated areas be retrofitted to include automatic irrigation.
- It is recommended that new irrigation systems be designed to include remote controller operation through the use of radio control systems.

COMMUNITY FORUM APPENDIX

APPENDIX

FIRST COMMUNITY FORUM MAIN POINTS

- **Summit to become hotel again**
- Address skate board issue
- “Movie in the Park”
- History on the buildings with sign system.
- Welcome Center/Chamber tied to Downtown
- City tax break to encourage business to relocate to Downtown
- **Improve / establish gateways to Downtown (Monuments)**
 - Inman Street / First Street
 - Wildwood Street / Inman Street
- Public Transportation/trolley depot as Hub, linked to City.
- **Performing Arts Center**
- **Maytag as anchor to Downtown**
- Court House “re-skinned” and brought back to former glory
- Downtown alive after 5pm
- Relocate Court House to another segment of city. Replace with housing
- **Angled parking at Square**
- One way around Square
- Shopping district (quaint character)
- Better access from I-75 (new exit?)
- Place for bikes / skaters Downtown (bike friendly)
- Permanent seating and storage for Festivals
- **8th Street / Ocoee Street Monument Park**
- **Woolen Mill / developed as attraction.**
- Central Avenue connect to Keith Street
- More pull-in parking at Courthouse
- Brick sidewalks
- **Plaza near Library**
- **1st Street Greenway link to Village Green**
- Improve Interstate signage

FIRST COMMUNITY FORUM FEEDBACK

Group One

Streets

- Additional angled parking around Square
- Maintain one-way around Square
- Signage: park and walk
- Adequate/better lighting for pedestrians
- Existing parking underutilized, need better signage

Parks and Plazas

- Plaza across from museum center
- Johnston Park “passive”

Linkages

- Better wayfinding - clear signage

New Development

- Downtown hotel/inn
- Music/entertainment/dining
- Quaint shops
- Mixed use - commercial/residential

Group Two

Streets

- Link/access to I-75
- Encourage bike traffic (non-vehicular)
- Inman: reduce speed, increase green (islands/medians), add sidewalks near village green
- East/West traffic
- Inman / Second should be like Broad / Ocoee
 - Inman Street one way east
 - Second Street one way west

Parks and Plazas

- Johnston Park
 - Restrooms, seating
 - Enhance bandstand
 - Create park pavilion

- Create a plaza/park in Five Points

Attractions

- Movie Theater
- Sousa Museum at Craigmiles Opera House
- Hotel / B&B / Inn
- Re use mills - shopping, mixed use

APPENDIX

FIRST COMMUNITY FORUM FEEDBACK

Group Three

Streets

- Parking back on Inman Street
- Trolley route to downtown
- Improve west and east gateways on Inman Street
- Walking tours - well marked

Parks and Plazas

- More lights on buildings - trees
- Move Courthouse (downtown closes at 5pm)
- Re-skin Courthouse
- Park at woolen mills
- Water Feature / Fountain

New Developments

- Downtown hotel
- Residential Apartments
- B&B / Inn
- Markets / Drugstore

Attractions

- Movie Theater
- Train Depot (stores, restaurants)
- Redevelop hosiery mill
- Annual festival - Theme "Old Cleveland"
 - Extension of cruise in?

Group Four

Streets

- Go back to original pull-in parking
- Inman Street-more pedestrian friendly and beautified
- Brick streets and sidewalks
- Historical markers on streets and buildings
- Wayfinding / improved signage
- No one way streets

Parks and Plazas

- Entertainment
- User friendly / family friendly / pet friendly
- Attractions that will draw tourist to Downtown
- Parks used to reinforced heritage & history of the area.
- Tie the Chamber to Downtown to tie in with Welcome Center / Information Center
- Welcome Center hours should be user friendly

FIRST COMMUNITY FORUM FEEDBACK

Linkages

- User friendly pedestrian Link
- Greenway should be along Ocoee, Parker, Johnston Park, Village Green, Centenary

New Development

- Incentives from City & State to attract new investment in Downtown Area
- Business Downtown - grocery store, fresh market, video store, theater & play house, etc.
- Take people away from the north end to come to the Downtown Area.
- Hotel Downtown / Motel / Bed & Breakfast.

Attractions

- Theater in Downtown
- More entertainment venues
- Add restaurants
- "Movie in the Park"
- Add concerts/festivals
- Skate park for children and entertainment area
- Historical tour (homes and buildings)

Group Five

Parks and Plazas

- Johnston Park-Something to promote utilization. More family friendly
- Water feature for families
- Arnold School should be part of the plan

Linkages

- Greenway (loop - Parker Street / Museum / Village Green / 25th Street
- Better pedestrian light fixtures throughout Downtown

New Development

- Maytag is an anchor
- Get old hotel "back" (Cherokee Hotel)
- Lee University has set a tone - build on it architecturally
- Historic District. Review board for renovations - new buildings
- More diverse business-not all professional offices
- Pharmacy / drug store
- Court House must be "re-skinned"
- Arnold School should be part of the plan

Attractions

- Discovery Museum
- Planetarium
- Theater, Performing Arts, Johnston Stage too "Rinky-Dink"

APPENDIX

FIRST COMMUNITY FORUM FEEDBACK

Group Six

Streets

- More convenient parking
- Pull-in parking around square
- Pedestrian / traffic sharing a good idea
- Inman Street - widen sidewalks - direct traffic to other routes.
- Lighting improvements needed everywhere
- Consider brick sidewalks or pavers similar to bricks
- Parker Street needs pedestrian enhancement - one way towards Downtown
- All streets should be bicycle friendly
- Parks/Plazas
- Create plaza/walkway between museum & Johnston Park and Extreme Outdoors
- Create plaza close to Library

Linkages

- Connect with Red Clay - need better signage
- Create Harrison Pike interchange
- Connect Greenway thru 1st Street

New Development

- Housing - use upstairs of downtown buildings
- Mixed use retail
- Feasibility of converting Summit back to hotel

Attractions

- Do something with Depot - look at Dalton, GA
- Skateboard park

Group Seven

Streets

- Inman Street more pedestrian - two lanes / wider sidewalks / landscaping
- West end of Inman Street needs sidewalks
- Connect Central Street to Keith Street / Parks
- 1st Street Greenway - linkage with landscaping

Parks and Plazas

- Redevelop Jack's Cleaners / storage building as park / green parking lot
- Expand Johnston Park south & west
- Monument Park at corner of 8th Street and Ocoee Street
- New Development
- Hotel / Motel / Inn
- Adaptive re-use housing in Five Points area

FIRST COMMUNITY FORUM FEEDBACK

Attractions

- B&B
- Cruise-in best event
- (Increased Promotion)
- Promote redevelopment of mill
- Better ties / promotion with Lee University student orientation (at museum, Downtown)

Group Eight

Streets

- Street names easier to identify
- Streets more pedestrian friendly
- Slow traffic
- Inman Street more pedestrian friendly
- Better alignment of streets (especially Edwards at Inman)
- Easier to get around downtown - better signage
- Traffic calming - brick pavers at crosswalks
- Better lighting
- Enhance gateways to city

Parks and Plazas

- Parks with good lighting
- Potential park location - 1st Street, Edwards Street, Central Street

Linkages

- Public Transportation - trolley / shuttle / public parking

Development

- Downtown housing
- More restaurants/coffee shops
- Entertainment combined with restaurant
- Community Theater
- Arts Academy for younger children
- Restore depot
- Hotel downtown

625 S. Gay Street · Suite 310
Knoxville, Tennessee 37902
865-637-1100
865-637-1101 Fax

To: Cindy Jenkins, Jones Mgmt Services
201 Keith Street, Suite 80
Cleveland, TN 37364

Date: April 29, 2004

Project: Downtown Cleveland Master Plan By: Shauna Godlevsky, ASLA

Transmittal of:	For Your:
<input checked="" type="radio"/> Prints	<input type="radio"/> Approval
<input type="radio"/> Tracings	<input type="radio"/> Review & Comment
<input type="radio"/> Specifications	<input checked="" type="radio"/> Use
<input type="radio"/> Change Order	<input type="radio"/> Record
<input type="radio"/> Shop Drawings	<input type="radio"/>
<input type="radio"/> Product Information	
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